



One University Place Construction Update 4
01/15/16

One University Place construction continues on the front building. Due to the cold weather, most of the work is being done in the lower parking area which is heated. Electrical and concrete work is being done in this lower level. Concrete block is being installed for first floor exterior walls. The target date to have the front building enclosed is now mid-February. City building inspector Terry Goerdts has inspected and signed off on every required inspection. This building is expected to be completed in late summer, 2016.

Utilities work (storm sewer and sanitary sewer) is progressing on the west side. The large sanitary sewer piping for the front building has been installed on the North Side of the building. Mechanical and electrical hookups to that system will begin.

MMS Engineering has submitted [Public Improvements Plan](#) (attached) for review of a left turn lane on Melrose and Sunset/Melrose intersection improvements. Also attached to this report is the portion of the January 2016 [City Engineer's report](#) to City Council that relate to the OUP project. The City Engineer refers to the Public Improvement Plan in his report, and also specifies the timeline for review of the plans, public comment, and steps required before approval.

MEMORANDUM

TO: University Heights, Mayor, Council, and Staff
FROM: Josiah Bilskemper, P.E.
DATE: January 9, 2016
RE: City Engineer's Report

(3) One University Place – Public Improvements Project

- a. I communicated with Ron Amelon at MMS regarding the planned project schedule for the roadway and intersection improvements project.
 - i. He reports that an updated set of plans is to be submitted to the city for review in early January. I haven't received anything as of yet, but will continue to follow up with Mr. Amelon, and will forward to council when it is received.
 - ii. Mr. Amelon would like to have this project out to bidders in February, and receive bids in early March so that council could consider awarding a construction contract in March. The goal is to have the roadway and intersection work completed by August 1, 2016, which is the planned occupancy date for the OUP south building currently under construction.
 - iii. Once the plans and specifications are reviewed and completed, council will need to take several actions associated with the public bidding process. This includes "Setting a Public Hearing," "Holding a Public Hearing," and then reviewing the bids received and "Consideration of Awarding a Construction Contract" based on those bids. I anticipate a special council meeting would need to be held at some point to accommodate this proposed schedule of awarding a construction contract in March 2016.
 - iv. There will also be a need during this time for the developer to provide, and the council to take action on, required legal documentation for the acquisition of additional right-of-way on the north side of the intersection. This will accommodate the realigned street on the north side of the intersection.
- b. *The council approved a motion at the November 10, 2015 meeting to include replacement of several poor condition street panels on Melrose Avenue (adjacent to the One University Place frontage) as part of the One University Place Public Improvements Project being prepared by MMS Consultants. The City will be responsible for the construction costs associated with these additional panels that are outside the scope of work necessary for the OUP site construction. (Dec. Mtg.)*

ONE UNIVERSITY PLACE - PUBLIC IMPROVEMENTS

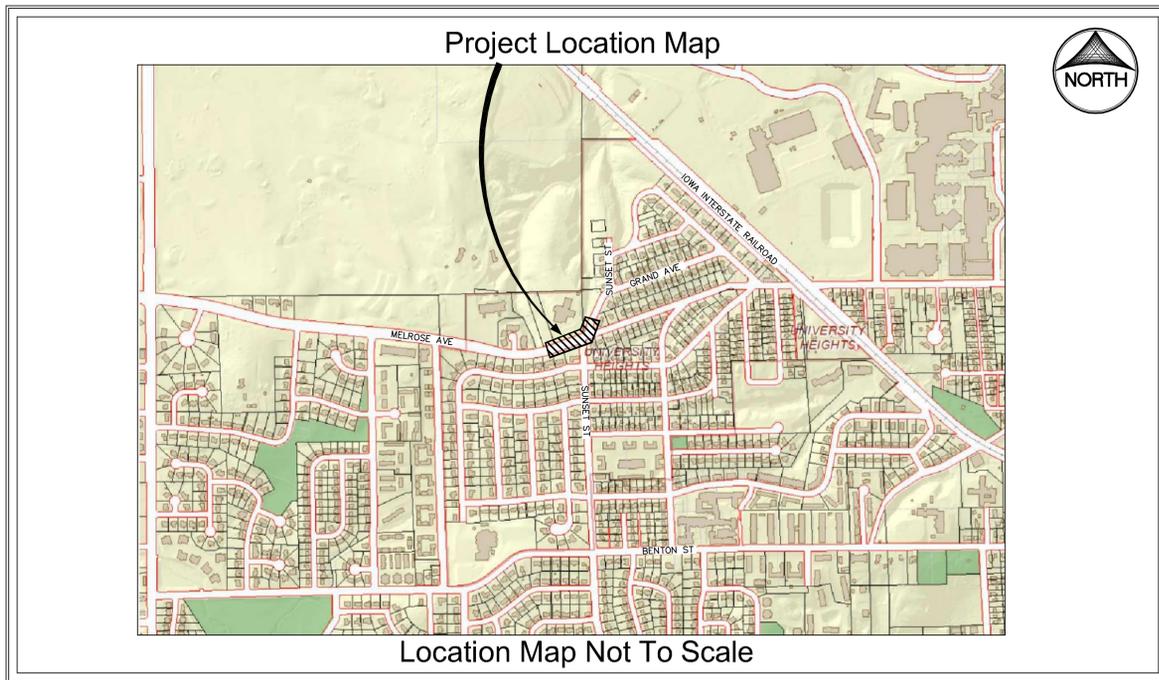
UNIVERSITY HEIGHTS, IOWA



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Date	Revision
08/07/15	90% CONSTRUCTION SET
10/27/15	PER CITY COMMENTS -JUR
11/04/15	PER CITY COMMENTS -JUR/MAK
01/08/15	PER CITY COMMENTS -PVA/MAK



STANDARD LEGEND AND NOTES	
-	PROPERTY &/or BOUNDARY LINES
-	CONGRESSIONAL SECTION LINES
-	RIGHT-OF-WAY LINES
-	EXISTING RIGHT-OF-WAY LINES
-	CENTER LINES
-	EXISTING CENTER LINES
-	LOT LINES, INTERNAL
-	LOT LINES, PLATTED OR BY DEED
-	PROPOSED EASEMENT LINES
-	EXISTING EASEMENT LINES
-	BENCHMARK
-	RECORDED DIMENSIONS
-	CURVE SEGMENT NUMBER
-	POWER POLE
-	POWER POLE W/DROP
-	POWER POLE W/TRANS
-	POWER POLE W/LIGHT
-	GUY POLE
-	LIGHT POLE
-	SANITARY MANHOLE
-	FIRE HYDRANT
-	WATER VALVE
-	DRAINAGE MANHOLE
-	CURB INLET
-	FENCE LINE
-	EXISTING SANITARY SEWER
-	PROPOSED SANITARY SEWER
-	EXISTING STORM SEWER
-	PROPOSED STORM SEWER
-	WATER LINES
-	ELECTRICAL LINES
-	TELEPHONE LINES
-	GAS LINES
-	CONTOUR LINES (1' INTERVAL)
-	PROPOSED GROUND
-	EXISTING TREE LINE
-	EXISTING DECIDUOUS TREE & SHRUB
-	EXISTING EVERGREEN TREES & SHRUBS

THE ACTUAL SIZE AND LOCATION OF ALL PROPOSED FACILITIES SHALL BE VERIFIED WITH CONSTRUCTION DOCUMENTS, WHICH ARE TO BE PREPARED AND SUBMITTED SUBSEQUENT TO THE APPROVAL OF THIS DOCUMENT.

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6.	DIMENSION PLAN
7.	PAVEMENT JOINTING PLAN
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15.	TRAFFIC CONTROL PLAN: WESTBOUND DETOUR ROUTE FOR PHASES 2 AND 3

BENCHMARK INFORMATION	
BM NO. 1	CUT "X" SET ON THE BACK OF A 4" CONCRETE SIDEWALK, LOCATED IN THE SOUTH RIGHT OF WAY OF MELROSE AVENUE, IN LINE WITH THE EAST BACK OF CURB LINE OF BIRKDALE COURT, AND 22.8± EAST/NORTHEAST OF A POWER POLE WITH A TRANSFORMER. ELEVATION: 776.60 (NAVD 88) DATUM
BM NO. 2	CUT "X" SET IN THE MIDDLE OF AN INTAKE, AT THE BACK OF CURB LINE, SAID INTAKE IS LOCATED ON THE NORTH EDGE OF MELROSE AVENUE, 52± WEST OF THE ENTRANCE TO ST. ANDREWS CHURCH, 1300 MELROSE AVENUE, IOWA CITY. ELEVATION: 779.55 (NAVD 88) DATUM

HORIZONTAL CONTROL			
POINT	NORTHING	EASTING	DESCRIPTION
H1	1947.03	1751.85	SEE BENCHMARK NO. 1 DESCRIPTION
H2	2010.77	2204.91	CUT "X" SET ON THE FACE OF A 4" CONCRETE SIDEWALK, LOCATED IN THE SOUTH RIGHT OF WAY OF MELROSE AVENUE, WHERE SAID SIDEWALK TAKES SOUTH TO STAY PARALLEL WITH THE RIGHT TURN LANE, 112.8± EAST OF THE ENTRANCE TO ST. ANDREWS CHURCH, 1300 MELROSE AVENUE, IOWA CITY.
H3	2415.97	2573.71	CUT "X" SET IN THE MIDDLE OF A 4" CONCRETE SIDEWALK, LOCATED IN THE SOUTH RIGHT OF WAY OF SUNSET STREET, SAID "X" BEING 203.9± NORTH OF THE NORTH EDGE OF A DRIVE FOR A RESIDENCE WITH AN ADDRESS OF 104 SUNSET STREET, IOWA CITY.

UTILITIES
THE CONTRACTOR SHALL NOTIFY IOWA ONE CALL NO LESS THAN 48 HRS. IN ADVANCE OF ANY DIGGING OR EXCAVATION.

WHERE PUBLIC UTILITY FIXTURES ARE SHOWN AS EXISTING ON THE PLANS OR ENCOUNTERED WITHIN THE CONSTRUCTION AREA, IT SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR TO NOTIFY THE OWNERS OF THOSE UTILITIES PRIOR TO THE BEGINNING OF ANY CONSTRUCTION. THE CONTRACTOR SHALL AFFORD ACCESS TO THESE FACILITIES FOR NECESSARY MODIFICATION OF SERVICES. UNDERGROUND FACILITIES, STRUCTURES AND UTILITIES HAVE BEEN PLOTTED FROM AVAILABLE SURVEYS AND RECORDS, AND THEREFORE THEIR LOCATIONS MUST BE CONSIDERED APPROXIMATE ONLY. IT IS POSSIBLE THERE MAY BE OTHERS, THE EXISTENCE OF WHICH IS PRESENTLY NOT KNOWN OR SHOWN. IT IS THE CONTRACTOR'S RESPONSIBILITY TO DETERMINE THEIR EXISTENCE AND EXACT LOCATION AND TO AVOID DAMAGE THERETO. NO CLAIMS FOR ADDITIONAL COMPENSATION WILL BE ALLOWED TO THE CONTRACTOR FOR ANY INTERFERENCE OR DELAY CAUSED BY SUCH WORK.

I hereby certify that this engineering document was prepared by me or under my direct supervision and that I am a duly licensed Professional Engineer under the laws of the State of Iowa.

RONALD L. AMELON, P.E. Iowa Lic. No. 14201

My commission expires on December 31, 20____.

Pages of sheets covered by this seal: _____

SEAL

COVER SHEET

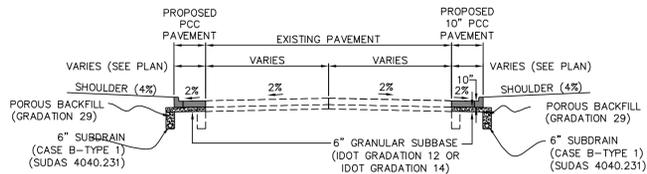
ONE UNIVERSITY PLACE
PUBLIC IMPROVEMENTS

UNIVERSITY HEIGHTS
JOHNSON COUNTY
STATE OF IOWA

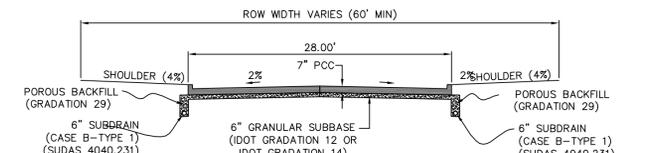
MMS CONSULTANTS, INC.

Date: 7-27-2015

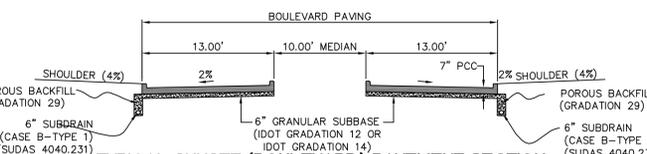
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Drawn By: MAK	Scale: N/A
Checked By: RLA	Sheet No: 1
Project No: IOWA CITY 5136012	of 15



TYPICAL MELROSE PAVEMENT SECTION
N.T.S.



TYPICAL SUNSET (NON BOULEVARD) PAVEMENT SECTION
N.T.S.



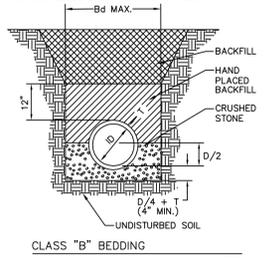
TYPICAL SUNSET (BOULEVARD) PAVEMENT SECTION
N.T.S.

STORM SEWER CONSTRUCTION NOTES

- CITY OF UNIVERSITY HEIGHTS DESIGN AND CONSTRUCTION STANDARDS AND PROCEDURES PREVAIL. ALL MANHOLE AND INTAKE CASTINGS SHALL HAVE THE WORDS "STORM SEWER" CAST INTO THE LID.
- ALL STORM SEWERS SHALL BE CLASS 3 RCP UNLESS NOTED OTHERWISE IN THE PLANS.
- AT PLACES WHERE A FLARED END SECTION IS REQUIRED, PIPE LENGTH INCLUDES THE FLARED END. THE LAST TWO JOINTS ARE TO BE TIED WHERE FLARED END SECTIONS ARE REQUIRED.
- ALL STORM SEWERS SHALL BE PROVIDED WITH CLASS "B" BEDDING, UNLESS NOTED OTHERWISE.
- STORM SEWER TRENCHES SHOWN ON THE PROFILE VIEW SHALL BE BACKFILLED WITH EITHER OF THE FOLLOWING COMPACTED TO 95% STANDARD PROCTOR DENSITY:
 - SUITABLE EXCAVATED MATERIAL. IF EXCAVATED MATERIAL IS NOT SUITABLE, THEN
 - CRUSHED STONE AS SPECIFIED FOR GRANULAR TRENCH BACKFILL SHALL BE USED.
- GRANULAR TRENCH BACKFILL SHALL BE CRUSHED STONE CONFORMING TO I.D.O.T. STANDARD SPECIFICATION 4120.04 WITH 1" MAXIMUM AGGREGATE SIZE. COMPACT TO 95% STANDARD PROCTOR DENSITY.
- THE ENTIRE DEPTH OF TRENCH FOR STORM SEWERS UNDER PUBLIC STREET PAVEMENT SHALL BE BACKFILLED WITH CLASS "A" CRUSHED STONE.
- ALL STORM SEWERS SHALL HAVE CONFINED "O" RING GASKETS OR PROFILE GASKETS. STORM SEWERS 36" AND SMALLER SHALL HAVE BELL AND SPIGOT JOINTS. STORM SEWERS LARGER THAN 36" MAY HAVE TONGUE AND GROOVE JOINTS. NO MASTIC JOINTS ALLOWED.
- ALL PIPE SHALL BE CERTIFIED.
- ALL STORM INTAKES SHALL BE A MINIMUM OF 48 INCHES FROM TOP OF CURB/RIM TO SUBGRADE. IF INVERT ELEVATIONS ARE INSUFFICIENT TO PROVIDE THIS REQUIRED DEPTH, THE CONTRACTOR TO PROVIDE DEEPER STRUCTURE AND POUR CONCRETE FILLET IN INTAKE TO MAKE INTAKE PIPES DRAIN AT INVERT ELEVATIONS LISTED.
- LIFT HOLES IN STORM SEWER WILL NOT BE ALLOWED.
- PROVIDE CONCRETE FILLETS IN ALL NEW & EXISTING DRAINAGE STRUCTURES PER REFERENCED DETAILS.
- CONTRACTOR SHALL PROVIDE A 5 YEAR MAINTENANCE BOND FOR ALL PUBLIC STORM SEWER.

WHERE PUBLIC UTILITY FIXTURES ARE SHOWN AS EXISTING ON THE PLANS, OR ENCOUNTERED WITHIN THE CONSTRUCTION AREA, IT SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR TO NOTIFY THE OWNERS OF THOSE UTILITIES PRIOR TO THE BEGINNING OF ANY CONSTRUCTION. THE CONTRACTOR SHALL AFFORD ACCESS TO THOSE FACILITIES FOR NECESSARY MODIFICATION OF SERVICES. UNDERGROUND FACILITIES, STRUCTURES AND UTILITIES HAVE BEEN PLOTTED FROM AVAILABLE SURVEYS AND RECORDS, AND THEREFORE THEIR LOCATIONS MUST BE CONSIDERED APPROXIMATE ONLY. IT IS POSSIBLE THAT THERE MAY BE OTHER FACILITIES IN THE CONSTRUCTION AREA, THE EXISTENCE OF WHICH IS NOT PRESENTLY KNOWN OR SHOWN HEREON. IT IS THE CONTRACTOR'S RESPONSIBILITY TO DETERMINE THEIR EXISTENCE AND EXACT LOCATION, AND TO AVOID DAMAGE THERETO. NO CLAIMS FOR ADDITIONAL COMPENSATION WILL BE ALLOWED TO THE CONTRACTOR FOR ANY INTERFERENCE OR DELAY CAUSED BY SUCH WORK.

- NOTES:
- PIPE SHALL BE PLACED ON CRUSHED STONE MATERIAL.
 - BELL HOLES SHALL BE HAND SHAPED SO THAT ONLY PIPE BARREL RECEIVES BEARING PRESSURE.
 - PLACE BEDDING TO ENSURE THAT THERE ARE NO VOIDS UNDER OR ALONGSIDE THE LENGTH OF PIPE.
 - BACK FILL SHALL BE HAND TAMPED UP TO 12" ABOVE TOP OF PIPE.
 - SEE TABLE FOR ALLOWABLE TRENCH WIDTH Bd.



ID INCHES	Bd FEET & INCHES
12	2'-3"
15	2'-9"
18	3'-0"
21	3'-6"
24	3'-6"
30	4'-3"
36	4'-9"
42	5'-6"

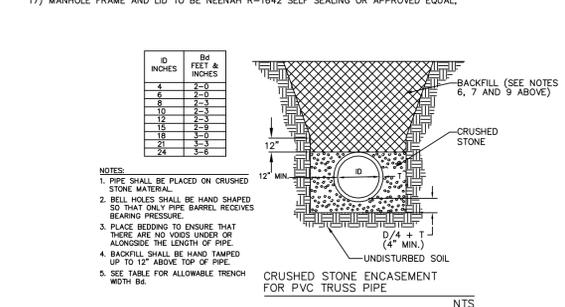
SANITARY SEWER AND WATER MAIN CONSTRUCTION NOTES

- SANITARY SEWER AND WATER MAIN CONSTRUCTION SHALL BE IN ACCORDANCE WITH THE PROJECT SPECIFICATIONS, TO MAKE UP THE DESIGN AND CONSTRUCTION STANDARDS AND PROCEDURES SHALL PREVAIL.
- CONTRACTOR SHALL PROVIDE CRUSHED STONE ENCASUREMENT BEDDING FOR ALL SANITARY SEWER UNLESS NOTED OTHERWISE ON THE PLANS.
- SANITARY SEWERS TO BE PVC TRUSS PIPE UNLESS NOTED OTHERWISE. SANITARY SEWER SERVICE LINES SHALL BE PVC, SDR 23.5 WITH GASKETED JOINTS.
- CONTRACTOR TO PROVIDE CLOW "BAND-SEAL" COUPLINGS FOR DISSIMILAR PIPE CONNECTIONS.
- 6" AND 8" DIAMETER WATER MAINS SHALL BE DR-18 PVC PIPE, EXCEPT UNDER PUBLIC ROADS AND ALONG SMALL RADIUS CURVES WHERE CLASS 52 DIP SHALL BE USED. UNLESS OTHERWISE NOTED ON THE PLANS, ALL FITTINGS FOR WATER MAIN SHALL BE RESTRAINED WITH BOTH MEGA-LUGS AND THRUST BLOCKS.
- GRANULAR TRENCH BACKFILL SHALL BE CLASS A CRUSHED STONE CONFORMING TO I.D.O.T. STANDARD SPECIFICATION 4120.04 WITH 1" MAXIMUM AGGREGATE SIZE. COMPACT TO 95% STANDARD PROCTOR DENSITY.
- WATER MAINS WITHIN STREET RIGHT OF WAYS OR WITHIN EASEMENTS ADJACENT TO THE STREET RIGHT OF WAYS SHALL BE BACKFILLED WITH EITHER OF THE FOLLOWING COMPACTED TO 95% STANDARD PROCTOR DENSITY:
 - SUITABLE EXCAVATED MATERIAL. IF EXCAVATED MATERIAL IS NOT SUITABLE, THEN
 - CRUSHED STONE AS SPECIFIED FOR GRANULAR TRENCH BACKFILL SHALL BE USED.
- THE ENTIRE DEPTH OF TRENCH FOR SANITARY SEWERS AND WATER MAINS LOCATED UNDER STREET PAVEMENT SHALL BE BACKFILLED WITH CLASS A CRUSHED STONE.
- ALL SANITARY SEWER SERVICE LINES CROSSING STREET RIGHT-OF-WAY SHALL BE BACKFILLED IN ACCORDANCE WITH THE PRECEDING NOTE.
- THE FOLLOWING MINIMUM CLEARANCES MUST BE MAINTAINED :
 - WATER MAIN SHALL BE LOCATED 10 FEET HORIZONTALLY DISTANT FROM ALL SANITARY SEWER AND STORM SEWER.
 - WATER MAIN SHALL NOT PASS THROUGH NOR CONTACT A SEWER OR A SEWER MANHOLE. A MINIMUM HORIZONTAL SEPARATION OF 3 FEET SHALL BE MAINTAINED.
 - VERTICAL SEPARATION OF WATER MAINS CROSSING OVER ANY SANITARY SEWER SHOULD BE A MINIMUM OF 18-INCHES, MEASURED OUTSIDE TO OUTSIDE FROM THE CLOSEST EDGE OF EACH PIPE. IF PHYSICAL CONDITIONS PROHIBIT THIS SEPARATION, THE WATER MAIN SHALL NOT BE PLACED CLOSER THAN 6-INCHES ABOVE A SEWER OR 18-INCHES BELOW A SEWER. THE SEPARATION DISTANCE SHALL BE THE MAXIMUM FEASIBLE IN ALL CASES, WHERE THE SEWER CROSSES OVER OR LESS THAN 18 INCHES BELOW A WATER MAIN ONE FULL LENGTH OF SEWER PIPE OF WATER MAIN MATERIAL SHALL BE LOCATED SO BOTH JOINTS ARE AS FAR AS POSSIBLE FROM THE WATER MAIN. THE SEWER AND WATER PIPES MUST BE ADEQUATELY SUPPORTED AND HAVE WATER TIGHT JOINTS. A LOW PERMEABILITY SOIL SHALL BE USED FOR BACKFILL MATERIAL WITHIN 10 FEET OF THE POINT OF CROSSING.
 - WHERE THE WATER MAIN CROSSES SEWER, ONE FULL LENGTH OF WATER PIPE SHALL BE LOCATED SO BOTH JOINTS ARE AS FAR AS POSSIBLE FROM THE SEWER. THE WATER AND SEWER PIPES MUST BE ADEQUATELY SUPPORTED AND HAVE WATER TIGHT JOINTS. A LOW PERMEABILITY SOIL SHALL BE USED FOR BACKFILL MATERIAL WITHIN 10-Feet OF THE POINT OF CROSSING.
 - NOMINAL DEPTH OF WATER MAIN = 5.5 FEET TO TOP OF PIPE.
- ALL SANITARY SEWER AND SERVICE LINES SHALL BE AIR TESTED TO PASS THE FOLLOWING TEST:

AIR TESTING

 - PERFORM FROM MANHOLE-TO-MANHOLE AFTER BACKFILL.
 - PLACE PNEUMATIC PLUGS: (1) SEALING LENGTH: EQUAL TO OR GREATER THAN PIPE DIAMETER, (2) CAPABLE OF RESISTING INTERNAL TEST PRESSURE WITHOUT EXTERNAL BRACING OR BLOCKING.
 - INTRODUCE LOW-PRESSURE AIR INTO SEALED LINE AND ACHIEVE INTERNAL AIR PRESSURE 4 PSIG GREATER THAN MAXIMUM PRESSURE EXERTED BY GROUNDWATER ABOVE PIPE INVERT.
 - LIMIT INTERNAL PRESSURE IN SEALED LINE BELOW 8 PSIG.
 - ALLOW 2 MINUTES MINIMUM FOR AIR PRESSURE TO STABILIZE. DISCONNECT LOW-PRESSURE AIR HOSE FROM CONTROL PANEL.
 - MINIMUM TIME FOR PRESSURE TO DROP FROM 3.5 TO 2.5 PSIG GREATER THAN MAXIMUM PRESSURE EXERTED BY GROUNDWATER ABOVE PIPE INVERT:

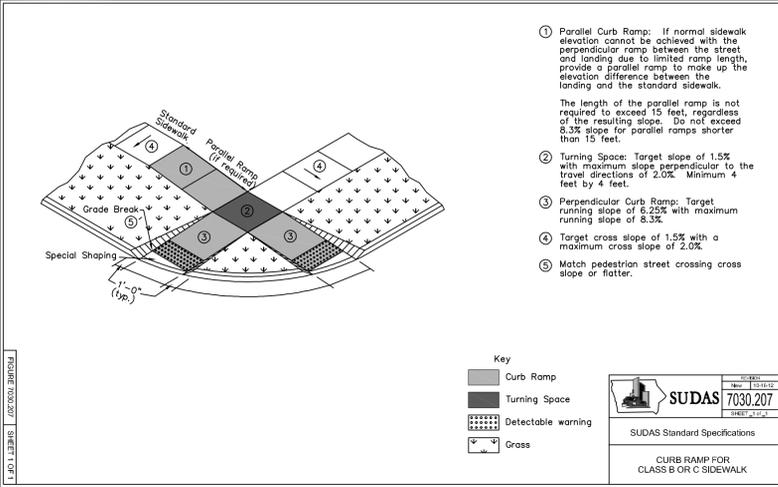
PIPE DIAMETER, INCHES	MINIMUM TIME, min:s	MINIMUM TIME, LONGER LENGTH, ft	TIME FOR
4	3:46	597	0.380 L
6	5:40	398	0.854 L
8	7:34	298	1.520 L
10	9:26	239	2.374 L
12	11:20	199	3.418 L
15	14:10	159	5.342 L
18	17:00	133	7.692 L
21	19:50	114	10.470 L
24	22:40	99	13.674 L
27	25:30	88	17.306 L
30	28:20	80	21.366 L
33	31:10	72	25.852 L
36	34:00	66	30.768 L
- IN AREAS WHERE GROUND WATER IS KNOWN TO EXIST, THE HEIGHT OF WATER ABOVE THE TOP OF THE PIPE BEING TESTED, IN FEET, SHALL BE DETERMINED AND THAT HEIGHT DIVIDED BY 2.3 TO ESTABLISH THE PRESSURE THAT WILL BE ADDED TO ALL READINGS ABOVE. ALTERNATIVELY, THE ENGINEER MAY ALLOW THE CONTRACTOR TO MEASURE INFILTRATION INTO THE SEWER BY USING A V-NOTCH WEIR OR OTHER SUITABLE DEVICE.
- LOCATE, REPAIR AND RETEST LEAKS.
- AIR TESTING SHALL BE CONSIDERED INCIDENTAL TO SANITARY SEWER CONSTRUCTION.
- ALL PVC TRUSS SEWERS SHALL HAVE A DEFLECTION TEST PERFORMED AS FOLLOWS:
 - DEFLECTION TEST SHALL BE CONDUCTED AFTER THE FINAL BACKFILL HAS BEEN IN PLACE AT LEAST 30 DAYS.
 - DEFLECTION TEST TO BE CONDUCTED USING A RIGID BALL OR MANDREL WITH A DIAMETER EQUAL TO 95% OF THE INSIDE DIAMETER OF THE PIPE. NO MECHANICAL PULLING DEVICES ALLOWED.
 - NO PIPE SHALL EXCEED A DEFLECTION OF 5%.
- MANHOLE FRAME AND LID TO BE NEENAH R-1642 SELF SEALING OR APPROVED EQUAL.



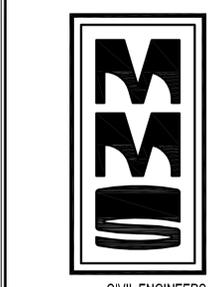
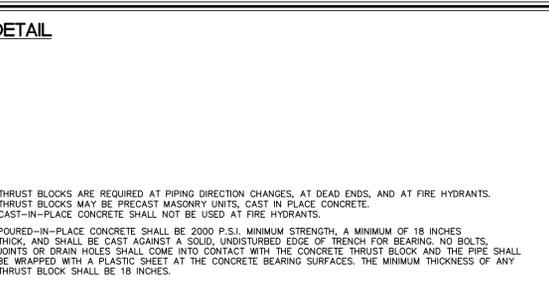
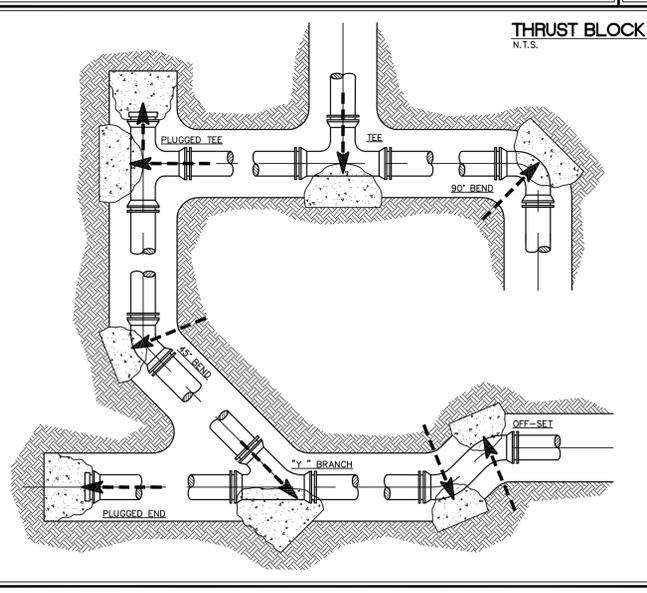
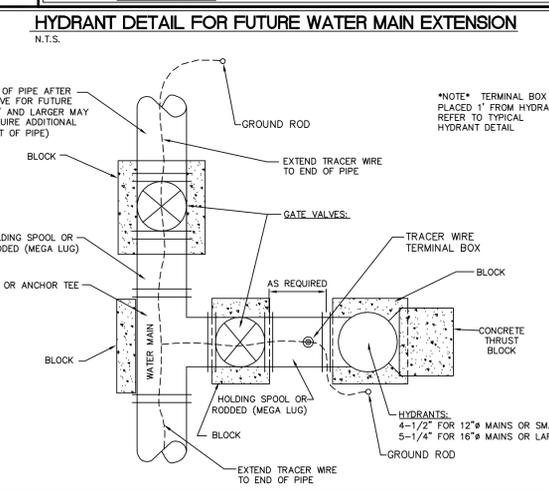
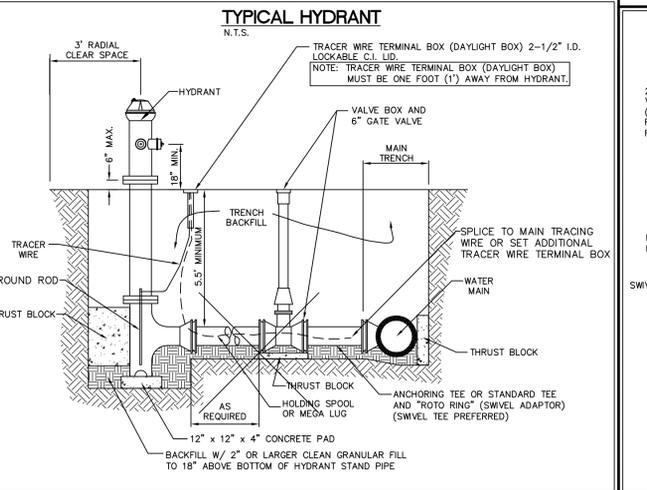
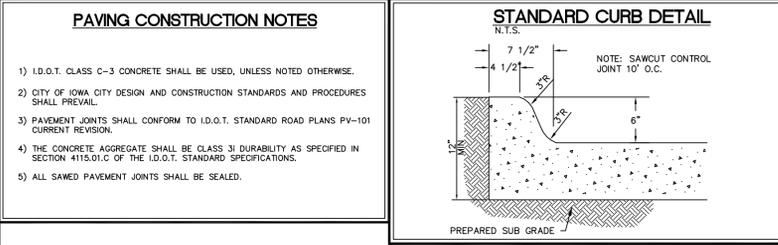
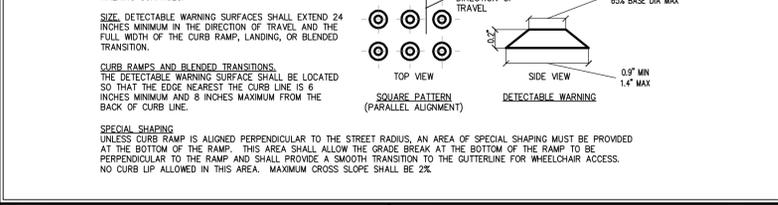
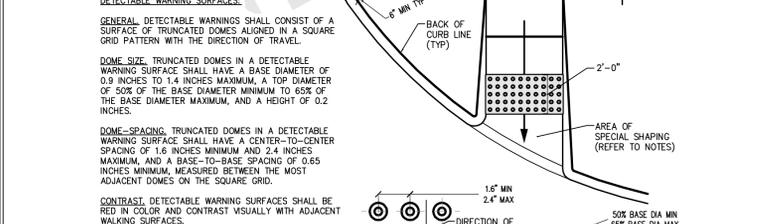
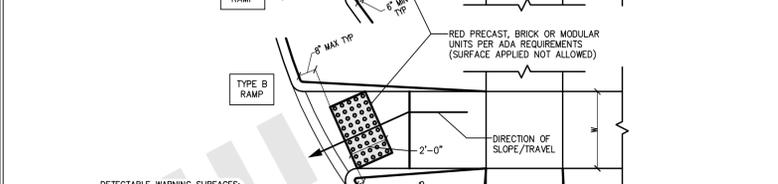
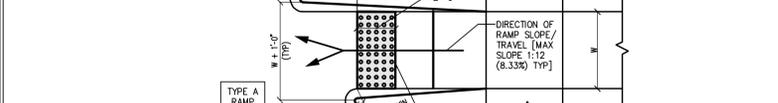
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THRUST BLOCK MINIMUM BEARING SURFACE AREA (IN SQUARE FEET)

PIPE SIZE	DEAD END OR TEE	90° BEND	45° BEND	11-1/4 OR 22-1/2 BEND
4"	3.0	4.0	2.0	1.5
6"	6.0	8.0	4.5	2.5
8"	10.0	14.0	7.5	4.0
10"	14.5	20.5	11.0	6.0
12"	20.5	29.0	16.0	8.0
14"	27.5	39.0	21.0	11.0
16"	35.5	50.5	27.5	14.0
20"	55.0	78.0	42.0	21.5
24"	78.5	111.0	60.0	30.5



- Parallel Curb Ramp: If normal sidewalk elevation cannot be achieved with the perpendicular ramp between the street and landing due to limited ramp length, provide a parallel ramp to make up the elevation difference between the landing and the standard sidewalk. The length of the parallel ramp is not required to exceed 15 feet regardless of the resulting slope. Do not exceed 8.3% slope for parallel ramps shorter than 15 feet.
- Turning Space: Target slope of 1.5% with maximum slope perpendicular to the travel directions of 2.0%. Minimum 4 feet by 4 feet.
- Perpendicular Curb Ramp: Target running slope of 6.25% with maximum running slope of 8.3%.
- Target cross slope of 1.5% with a maximum cross slope of 2.0%.
- Match pedestrian street crossing cross slope or flatter.



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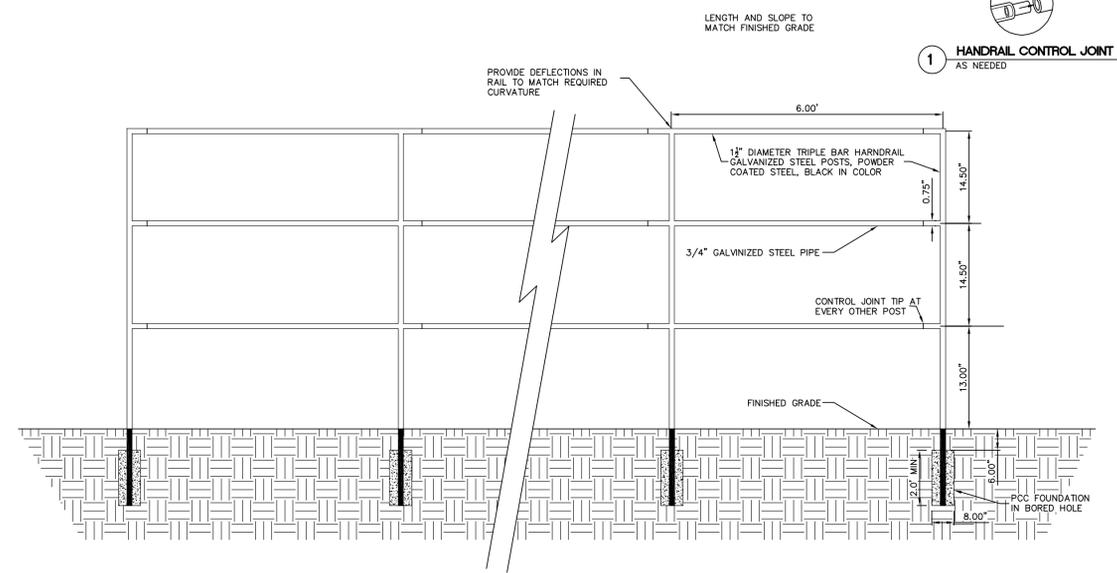
DETAILS

ONE UNIVERSITY PLACE PUBLIC IMPROVEMENTS

UNIVERSITY HEIGHTS
JOHNSON COUNTY
STATE OF IOWA

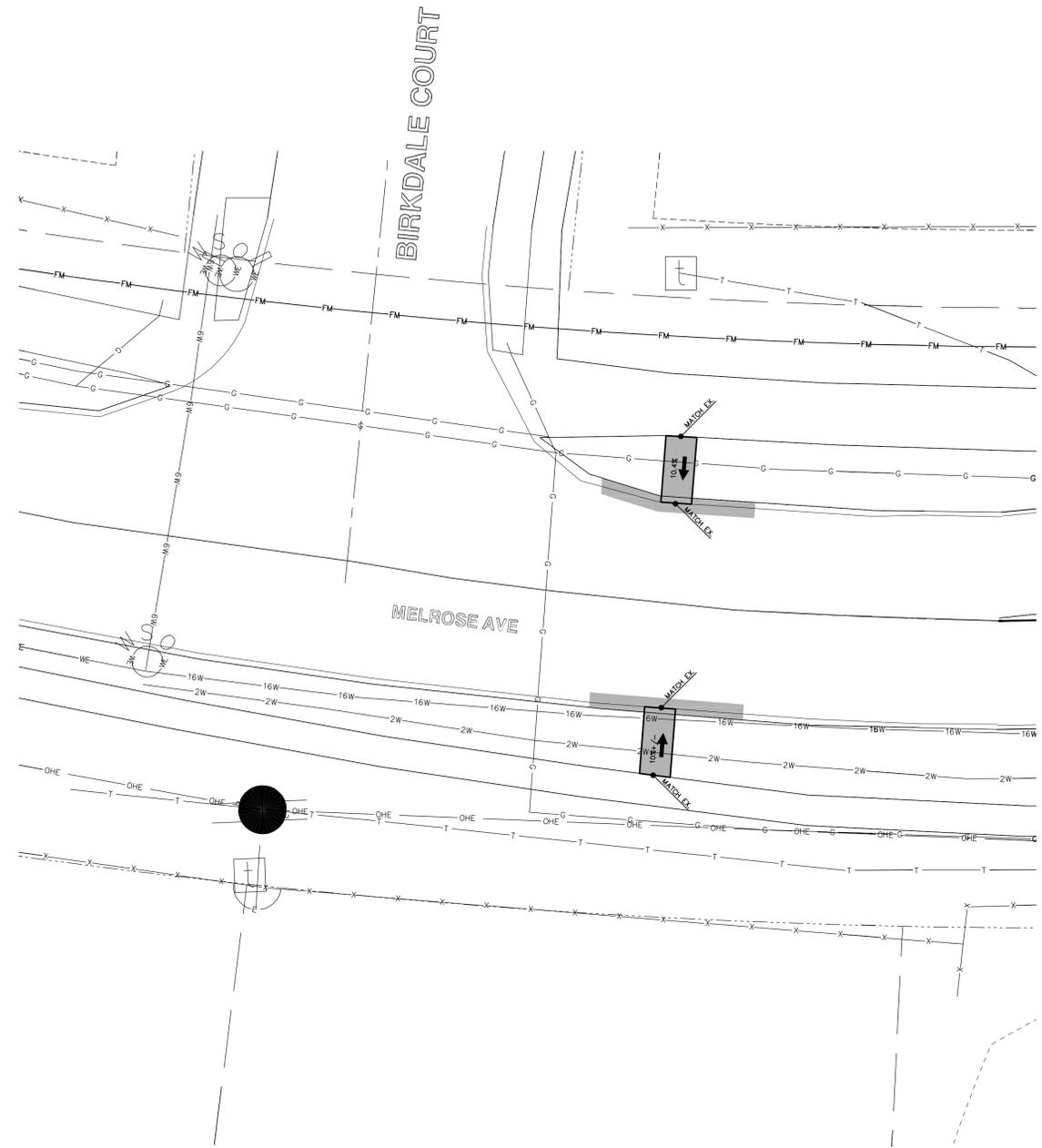
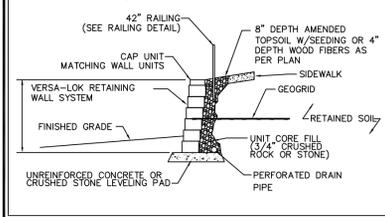
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Date: 7-27-2015
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Drawn by: MAK Scale: N/A
Checked by: RLA Sheet No:
Project No: IOWA CITY 5136012
2 of 15

HANDRAIL DETAIL
N.T.S.



1 HANDRAIL CONTROL JOINT
AS NEEDED

TYPICAL WALL SECTION
N.T.S.



OFFSITE TEMPORARY SIDEWALK GRADING
SCALE: 1"=10'



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DETAILS AND OFFSITE TEMPORARY SIDEWALK GRADING

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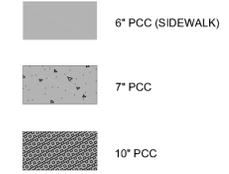
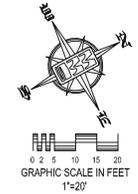
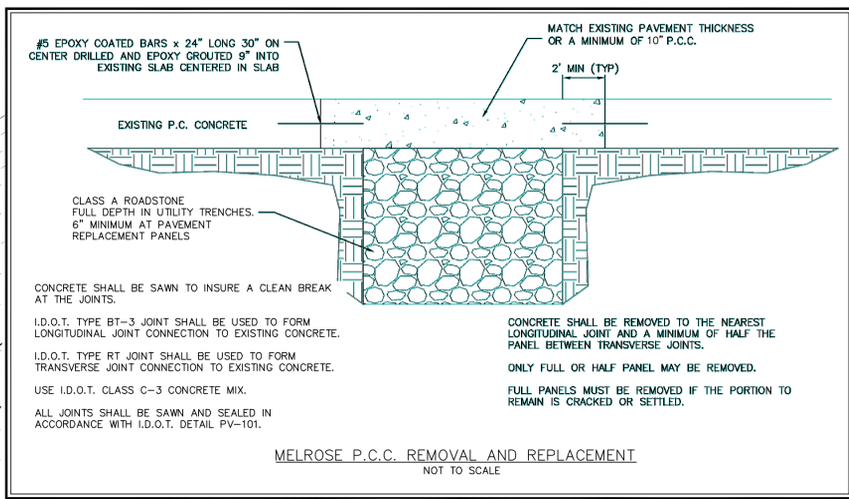
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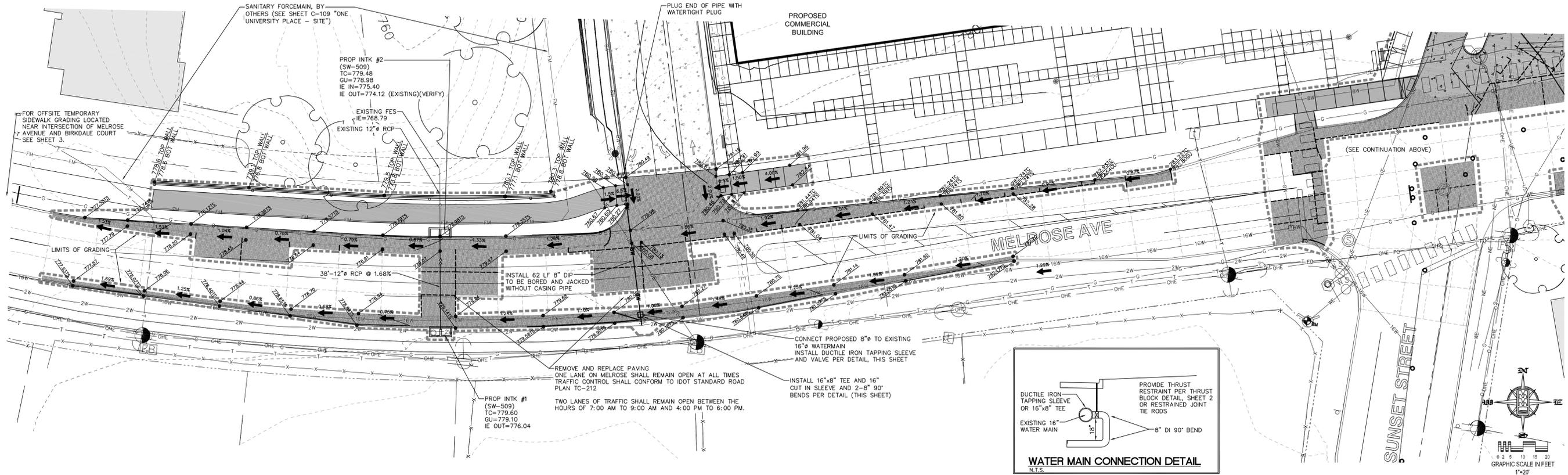
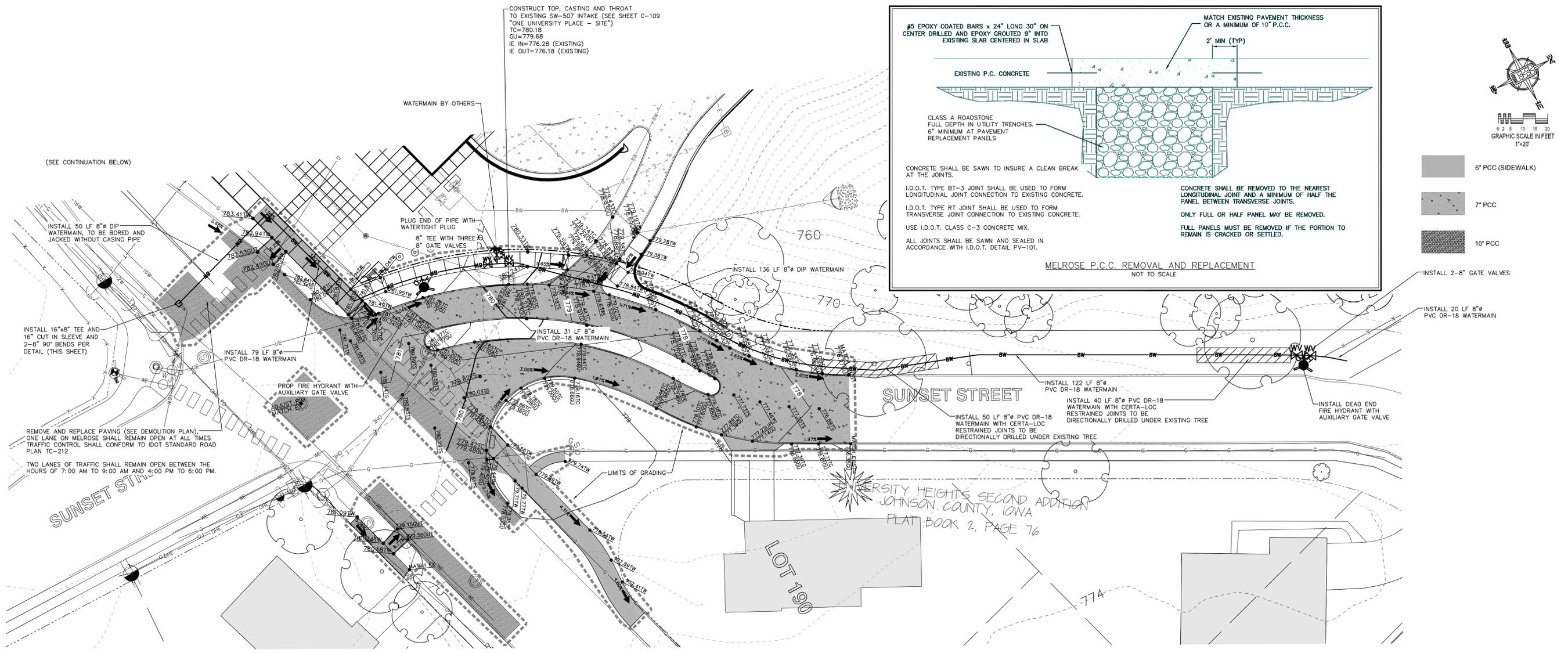
CONSTRUCT TOP, CASTING AND THROAT TO EXISTING SW-507 INTAKE (SEE SHEET C-109 "ONE UNIVERSITY PLACE - SITE")
 TC=780.18
 GU=779.68
 IE IN=776.28 (EXISTING)
 IE OUT=776.18 (EXISTING)



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GRADING AND UTILITY PLAN

ONE UNIVERSITY PLACE PUBLIC IMPROVEMENTS

UNIVERSITY HEIGHTS JOHNSON COUNTY STATE OF IOWA

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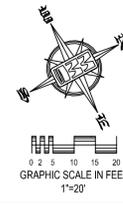
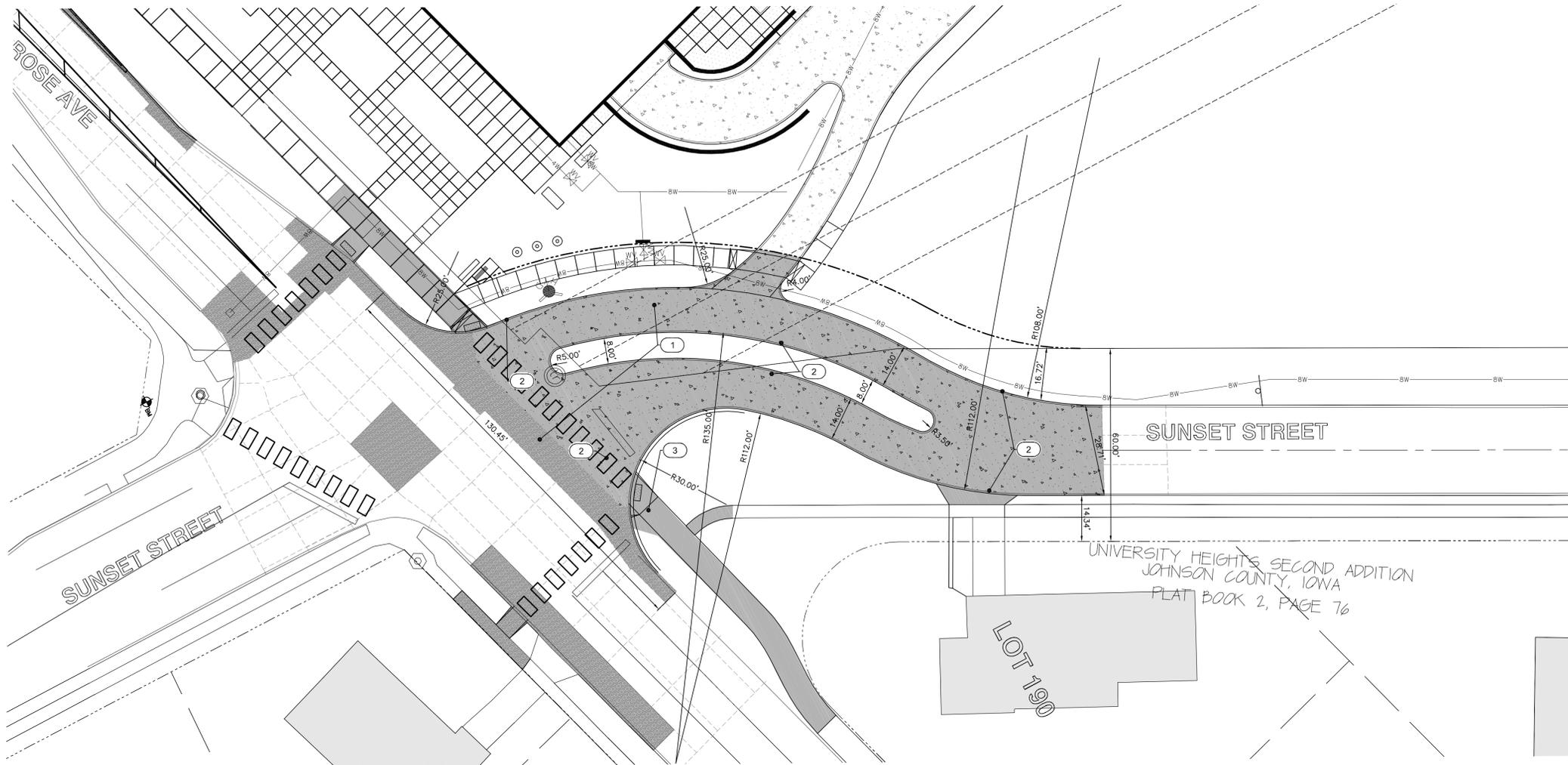
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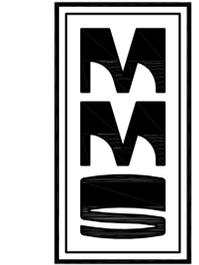
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Project No: IOWA CITY 5136012 of. 15



KEYNOTES		
NUMBER	KEYNOTE	DETAIL
1	INSTALL DRIVE PER DETAIL (THICKNESS AND MATERIAL AS NOTED)	
2	INSTALL 6" STANDARD CURB	
3	INSTALL 6" P.C.C. SIDEWALK PER MUNICIPAL DESIGN STANDARDS	
4	INSTALL CURB RAMP PER ADA REQUIREMENTS	
5	PROPOSED MODULAR BLOCK RETAINING WALL WITH HANDRAIL. (GEOTECHNICAL ENGINEER SHALL CERTIFY WALL PLANS PRIOR TO CONSTRUCTION)	

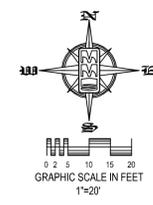
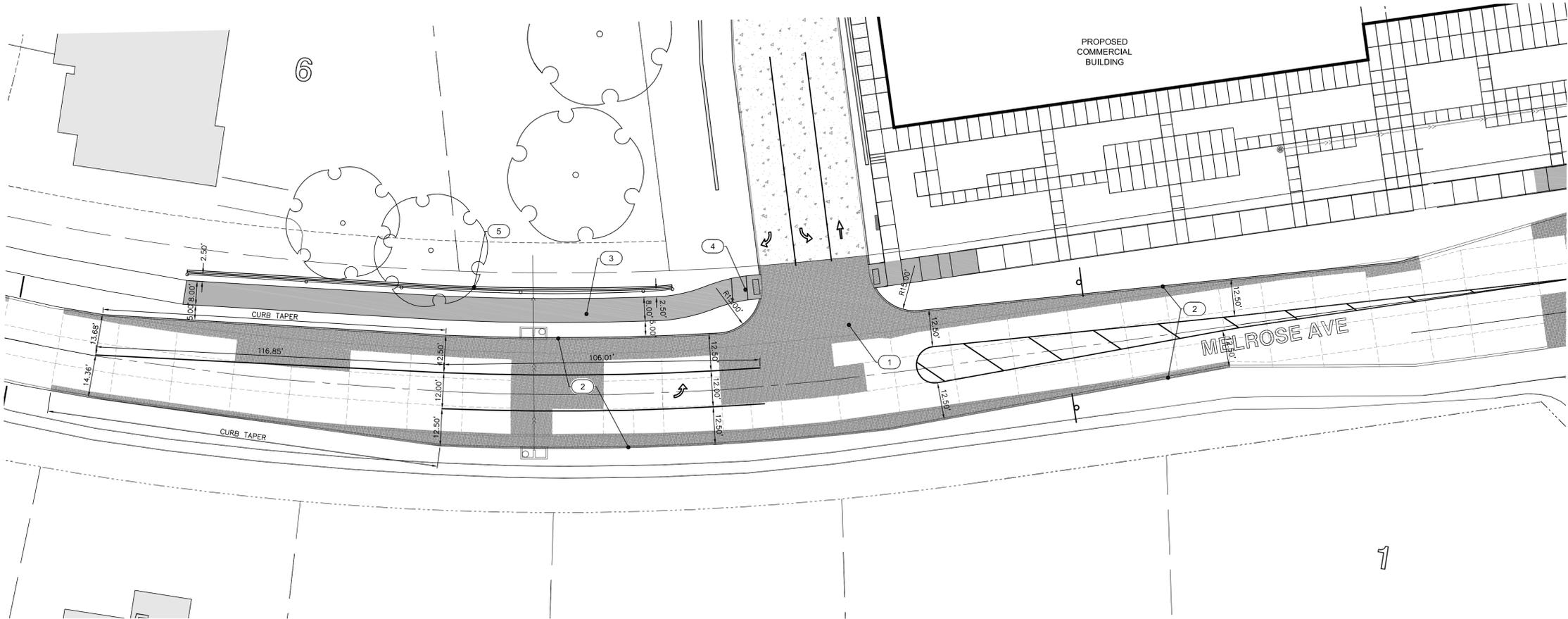
- 6" PCC (SIDEWALK)
- 7" PCC
- 10" PCC



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DIMENSION PLAN

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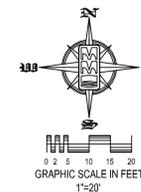
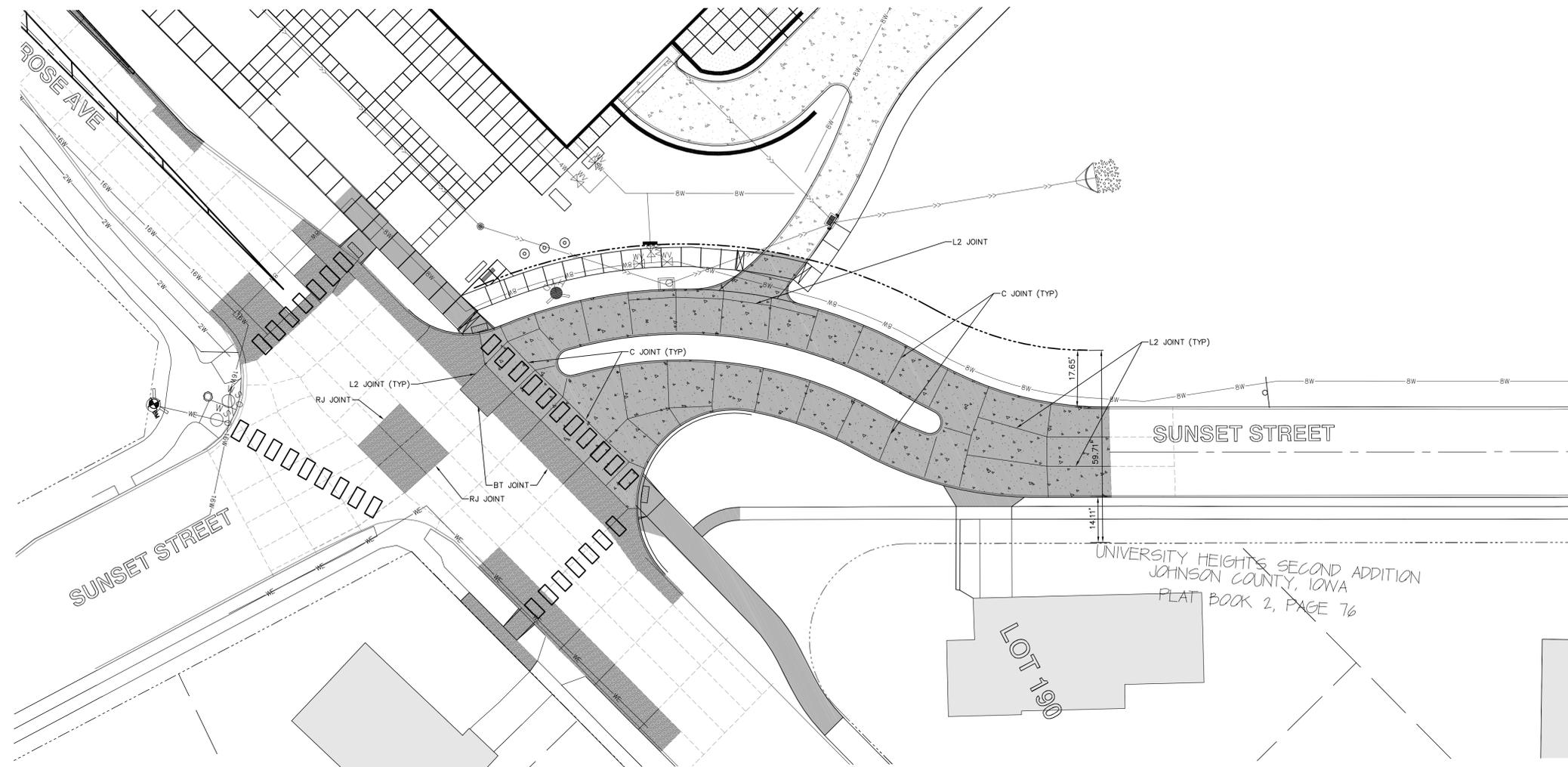
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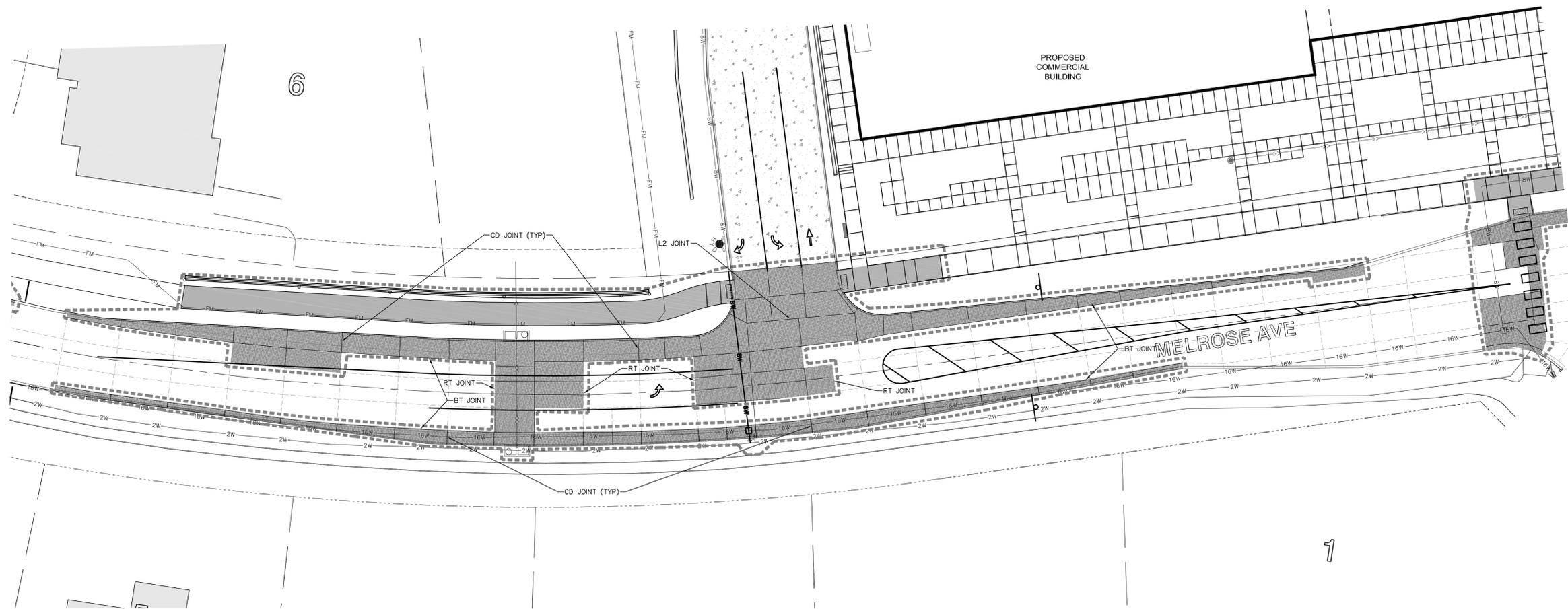
PAVEMENT
JOINTING
PLAN

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NOTE: ALL PAVEMENT MARKINGS SHALL BE EXOPY MARKINGS

STRIPING AND SIGNAGE PLAN

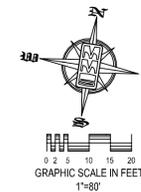
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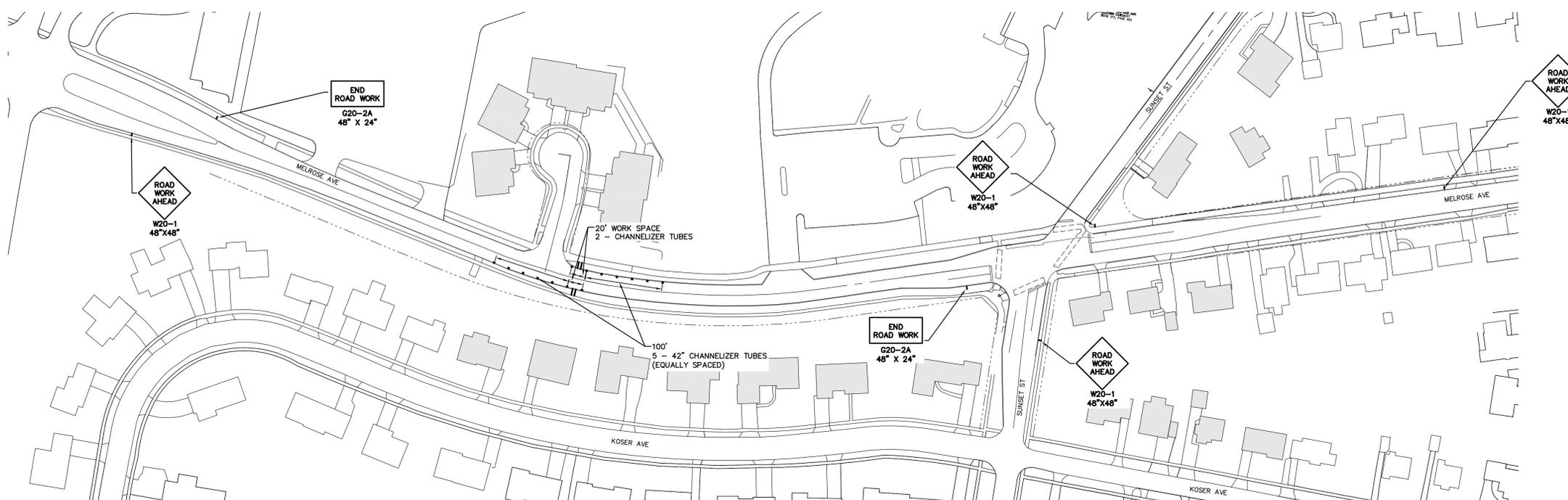
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- PHASE 1 NOTES:
- PHASE 1 SHALL BE COMPLETED PRIOR TO STARTING PHASE 2.
 - PHASE 7 MAY COMMENCE FOLLOWING COMPLETION OF PHASE 4.
 - TWO-WAY TRAFFIC ON MELROSE SHALL BE MAINTAINED AT ALL TIMES.
 - WORK MAY NOT OCCUR SIMULTANEOUSLY AT NORTH SIDE OF MELROSE AVENUE AND SOUTH SIDE OF MELROSE AVENUE.
 - TRAFFIC CONTROL FOR THIS STAGE SHALL CONFORM TO IDOT STANDARD ROAD PLAN TC-202.
 - TRAFFIC CONTROL MEASURES SHALL BE IN PLACE AT ALL TIMES.

PHASE 1
TEMPORARY SIDEWALK CROSSING CONSTRUCTION.
2 WAY TRAFFIC ON MELROSE AVENUE MAINTAINED.

PHASE 7
REMOVAL OF TEMPORARY SIDEWALK CROSSING.

TRAFFIC CONTROL PLAN:
PHASE 1 & 7

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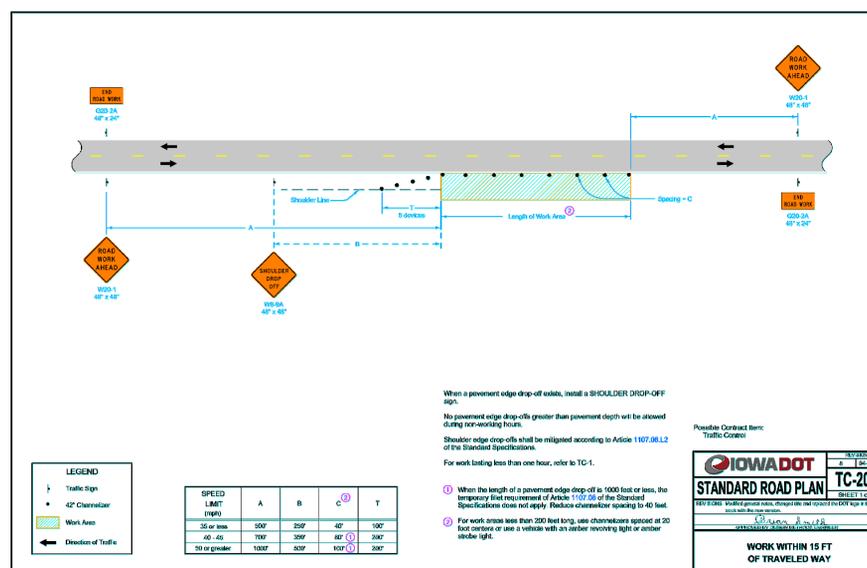
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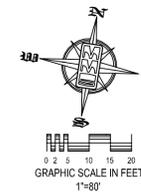
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GENERAL TRAFFIC CONTROL NOTES:

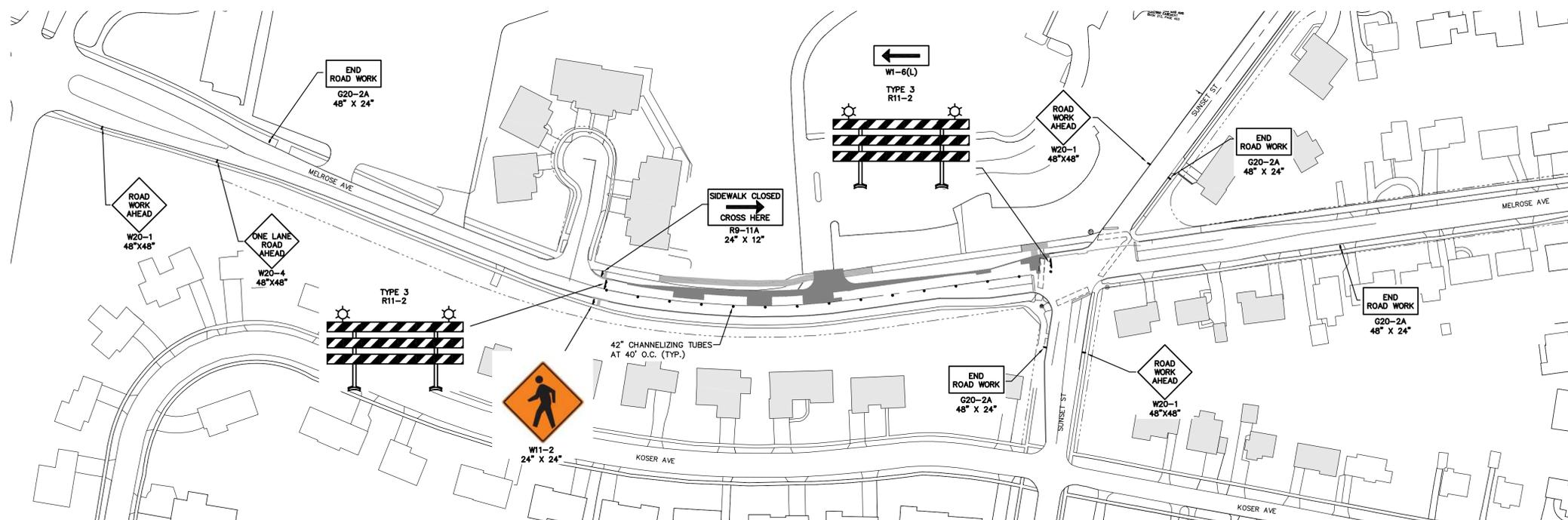
- TRAFFIC CONTROL FOR THIS PROJECT SHALL CONFORM TO THE MANUAL ON UNIFORM CONTROL DEVICES (MUTCD), LATEST VERSION.
- ALL TRAFFIC CONTROL DEVICES SHALL BE FURNISHED, ERECTED, MAINTAINED AND REMOVED BY THE CONTRACTOR.
- TRAFFIC CONTROL MEASURES SHALL BE IN PLACE AT ALL TIMES THAT THE CONTRACTOR IS PERFORMING WORK.
- A 'SIDEWALK CLOSED' SIGN ON A TYPE II BARRICADE SHALL BE POSTED ON SIDEWALKS PRIOR TO ANY PORTION OF REMOVED OR UNSUITABLE SIDEWALK.
- TEMPORARY PORTABLE SAFETY FENCE AS SPECIFIED IN SECTION 00.74.13 ITEM 10.E SHALL BE PLACED AROUND ANY OPEN TRENCH DURING NON WORKING HOURS. TEMPORARY PORTABLE SAFETY FENCE SHALL BE PLACED BETWEEN ANY PEDESTRIAN ROUTE AND THE CONSTRUCTION LIMITS DURING PERIODS OF CONSTRUCTION ACTIVITY. IT IS NOT REQUIRED THAT THE SAFETY FENCE BE INSTALLED AND MAINTAINED ALONG THE ENTIRE LENGTH OF THE PROJECT THROUGHOUT THE DURATION OF CONSTRUCTION.



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PHASE 2

WESTBOUND MELROSE AVENUE IMPROVEMENTS.
EASTBOUND MELROSE AVENUE TRAFFIC MAINTAINED.
WESTBOUND MELROSE AVENUE TRAFFIC FOLLOWS DETOUR ROUTE (SEE SHEET 15).

- PHASE 2 NOTES:
1. PHASE 1 SHALL BE COMPLETED PRIOR TO STARTING PHASE 2.
 2. PLACE TRAFFIC CONTROL DEVICES AS REQUIRED FOR PAVEMENT REMOVAL, UTILITY CONSTRUCTION, RETAINING WALL, SIDEWALK CONSTRUCTION AND REPLACEMENT OF NORTH PORTION OF MELROSE AVENUE.
 3. TRAFFIC CONTROL MEASURES SHALL BE IN PLACE AT ALL TIMES UNTIL WESTBOUND LANE IS ACCEPTED FOR TRAFFIC.

TRAFFIC CONTROL PLAN: PHASE 2

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PUBLIC IMPROVEMENTS

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GENERAL TRAFFIC CONTROL NOTES:

1. TRAFFIC CONTROL FOR THIS PROJECT SHALL CONFORM TO THE MANUAL ON UNIFORM CONTROL DEVICES (MUTCD), LATEST VERSION.
2. ALL TRAFFIC CONTROL DEVICES SHALL BE FURNISHED, ERECTED, MAINTAINED AND REMOVED BY THE CONTRACTOR.
3. TRAFFIC CONTROL MEASURES SHALL BE IN PLACE AT ALL TIMES THAT THE CONTRACTOR IS PERFORMING WORK.
4. COORDINATE WITH THE IOWA CITY TRAFFIC ENGINEERING SUPERINTENDENT (PUBLIC WORKS DEPARTMENT) TO MODIFY OPERATION OF THE TRAFFIC SIGNALS AT THE MELROSE AVENUE - SUNSET STREET INTERSECTION TO PROVIDE ALTERNATING TRAFFIC MOVEMENT IN THE EAST - WEST DIRECTIONS DURING THIS PHASE OF TRAFFIC CONTROL.
5. A "SIDEWALK CLOSED" SIGN ON A TYPE II BARRICADE SHALL BE POSTED ON SIDEWALKS PRIOR TO ANY PORTION OF REMOVED OR UNSUITABLE SIDEWALK.
6. TEMPORARY PORTABLE SAFETY FENCE AS SPECIFIED IN SECTION 00 74 13 ITEM 10.E SHALL BE PLACED AROUND ANY OPEN TRENCH DURING NON WORKING HOURS. TEMPORARY PORTABLE SAFETY FENCE SHALL BE PLACED BETWEEN ANY PEDESTRIAN ROUTE AND THE CONSTRUCTION LIMITS DURING PERIODS OF CONSTRUCTION ACTIVITY. IT IS NOT REQUIRED THAT THE SAFETY FENCE BE INSTALLED AND MAINTAINED ALONG THE ENTIRE LENGTH OF THE PROJECT THROUGHOUT THE DURATION OF CONSTRUCTION.

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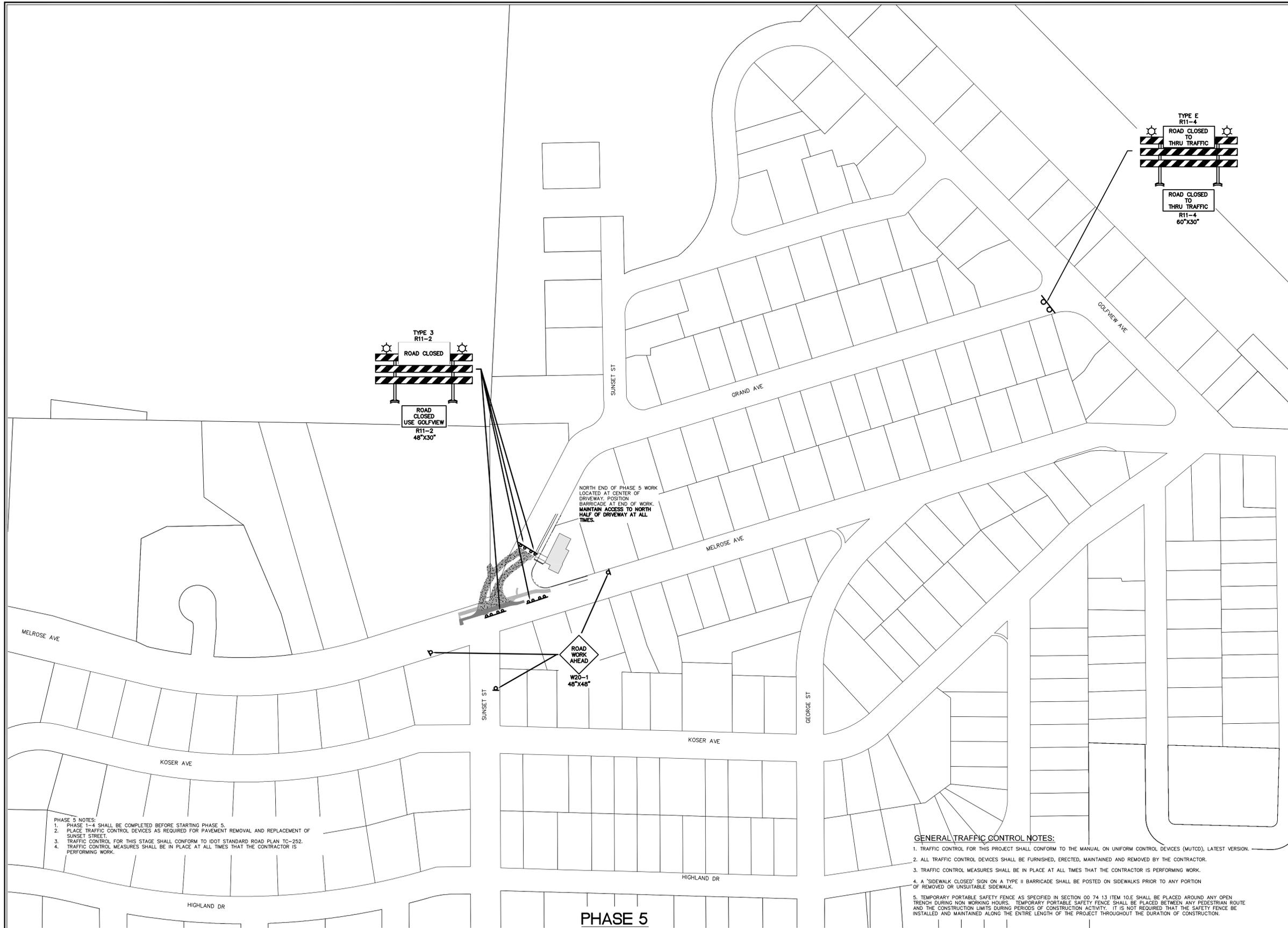
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PHASE 5 NOTES:
1. PHASE 1-4 SHALL BE COMPLETED BEFORE STARTING PHASE 5.
2. PLACE TRAFFIC CONTROL DEVICES AS REQUIRED FOR PAVEMENT REMOVAL AND REPLACEMENT OF SUNSET STREET.
3. TRAFFIC CONTROL FOR THIS STAGE SHALL CONFORM TO IDOT STANDARD ROAD PLAN TC-252.
4. TRAFFIC CONTROL MEASURES SHALL BE IN PLACE AT ALL TIMES THAT THE CONTRACTOR IS PERFORMING WORK.

GENERAL TRAFFIC CONTROL NOTES:
1. TRAFFIC CONTROL FOR THIS PROJECT SHALL CONFORM TO THE MANUAL ON UNIFORM CONTROL DEVICES (MUTCD), LATEST VERSION.
2. ALL TRAFFIC CONTROL DEVICES SHALL BE FURNISHED, ERECTED, MAINTAINED AND REMOVED BY THE CONTRACTOR.
3. TRAFFIC CONTROL MEASURES SHALL BE IN PLACE AT ALL TIMES THAT THE CONTRACTOR IS PERFORMING WORK.
4. A 'SIDEWALK CLOSED' SIGN ON A TYPE II BARRICADE SHALL BE POSTED ON SIDEWALKS PRIOR TO ANY PORTION OF REMOVED OR UNSUITABLE SIDEWALK.
5. TEMPORARY PORTABLE SAFETY FENCE AS SPECIFIED IN SECTION 00 74 13 ITEM 10.E SHALL BE PLACED AROUND ANY OPEN TRENCH DURING NON WORKING HOURS. TEMPORARY PORTABLE SAFETY FENCE SHALL BE PLACED BETWEEN ANY PEDESTRIAN ROUTE AND THE CONSTRUCTION LIMITS DURING PERIODS OF CONSTRUCTION ACTIVITY. IT IS NOT REQUIRED THAT THE SAFETY FENCE BE INSTALLED AND MAINTAINED ALONG THE ENTIRE LENGTH OF THE PROJECT THROUGHOUT THE DURATION OF CONSTRUCTION.

PHASE 5
SUNSET STREET PAVEMENT REMOVAL AND REPLACEMENT AND WATERMAIN EXTENSION

TRAFFIC CONTROL PLAN: PHASE 5

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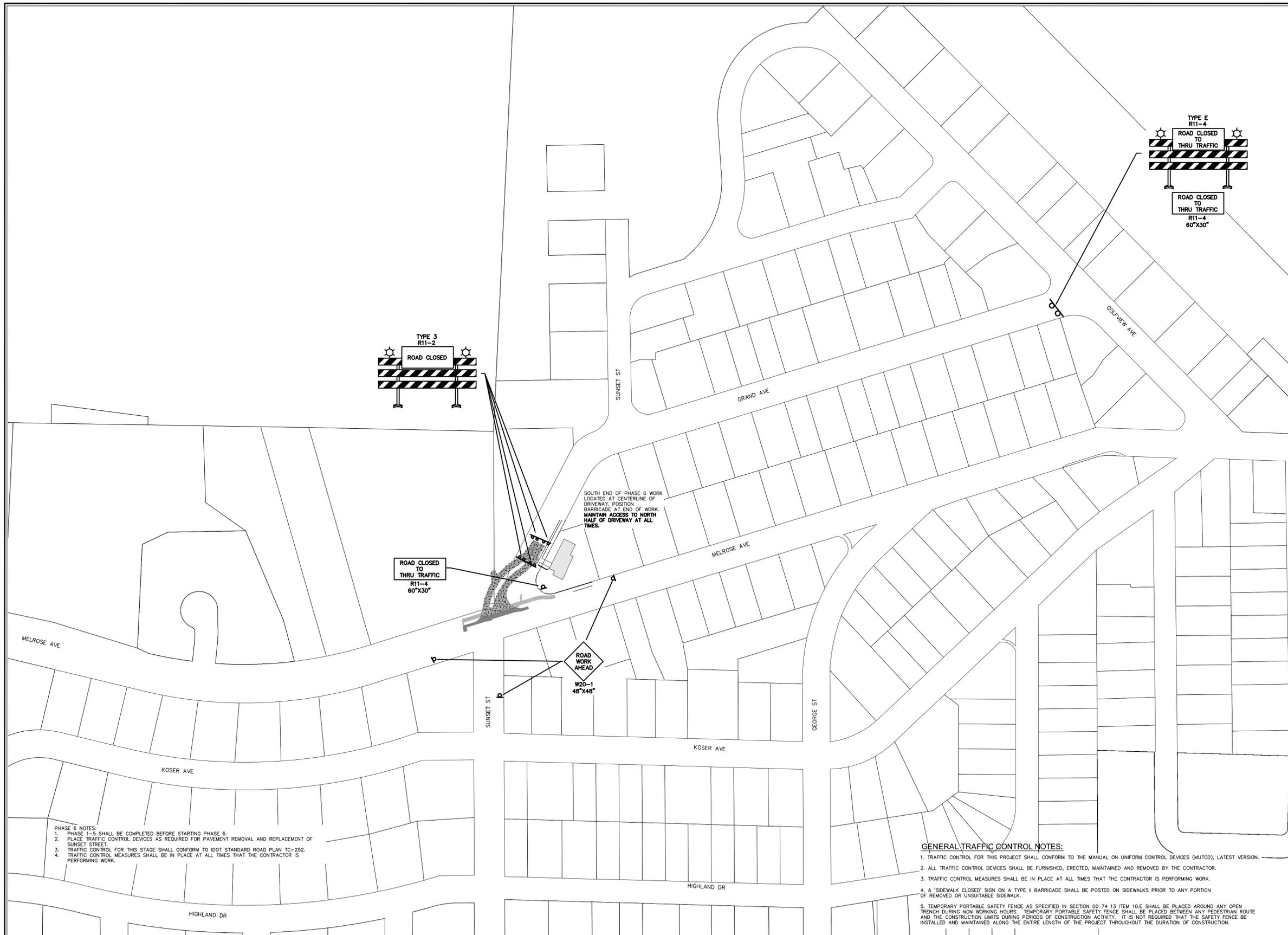


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PHASE 6
SUNSET STREET PAVEMENT REMOVAL AND REPLACEMENT AND WATERMAIN EXTENSION

TRAFFIC CONTROL PLAN: PHASE 6

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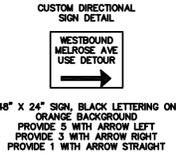
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DETOUR ROUTE

- DETOUR NOTES:**
- PHASE 1-4 SHALL BE COMPLETED BEFORE STARTING PHASE 5.
 - PLACE TRAFFIC CONTROL DEVICES AS REQUIRED FOR PAVEMENT REMOVAL AND REPLACEMENT OF SUNSET STREET.
 - TRAFFIC CONTROL FOR THIS STAGE SHALL CONFORM TO IDOT STANDARD ROAD PLAN TC-252.
 - TRAFFIC CONTROL MEASURES SHALL BE IN PLACE AT ALL TIMES THAT THE CONTRACTOR IS PERFORMING WORK.
- GENERAL TRAFFIC CONTROL NOTES:**
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**TRAFFIC CONTROL PLAN:
 WESTBOUND
 DETOUR ROUTE
 FOR PHASES 2
 AND 3**

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